



# 2025 UPDATED PAVEMENT MANAGEMENT PROGRAM (PMP)

PUBLIC WORKS  
GLENDDRA CITY COUNCIL  
DECEMBER 9, 2025

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# PAVEMENT MANAGEMENT PLAN

## City of Glendora 2025 – 2027 Goals and Action Plan

- Goal 4: Sustainable Infrastructure
- Action 4.1 Complete Pavement Management Program (PMP)

## 2025 Community Survey

- How can the City make Glendora a better place?:
- Improving Streets, roads and related infrastructure.

## PMP Update

- PMP Reports completed in 2020 and 2015
- 2025 PMP Update included in the FY 2024/25 Adopted Budget

## Agreement Awarded to Bucknam Infrastructure Group

- Prepared the 2020 PMP.

## Purpose

- Identify pavement conditions, maintenance needs & funding strategies
- Support strategic, data-based decision making for CIP selection.



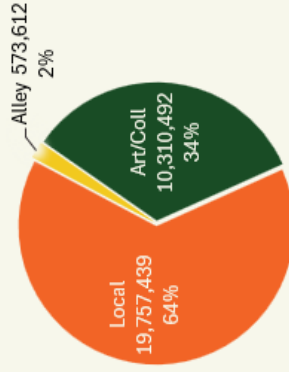
# OVERVIEW

- **Glendora Street Network**
- **How the PMP is Updated**
- **Pavement Condition Survey Results**
- **Funding Scenario Assessments**
- **Findings & Recommendations**
- **Next Steps**

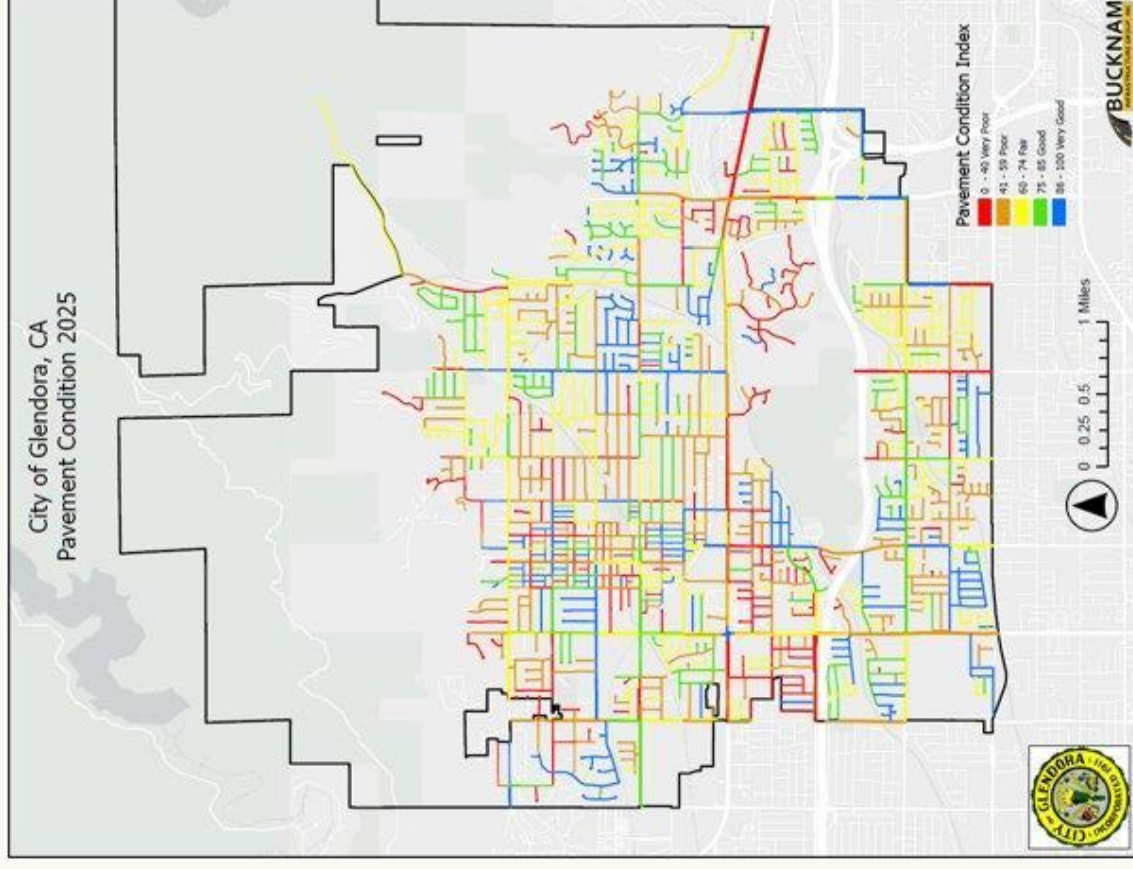
# GLENDORA STREET NETWORK

Type	Miles	Square Feet
Local / Residential	112.2	19,757,439
Arterial / Collector	45.0	10,310,492
Alleys	6.2	573,612
<b>Totals</b>	<b>163.4</b>	<b>30,641,543</b>

Pavement Area (SF) by Functional Class



**Asset Reconstruction Value = \$439.1 Million**

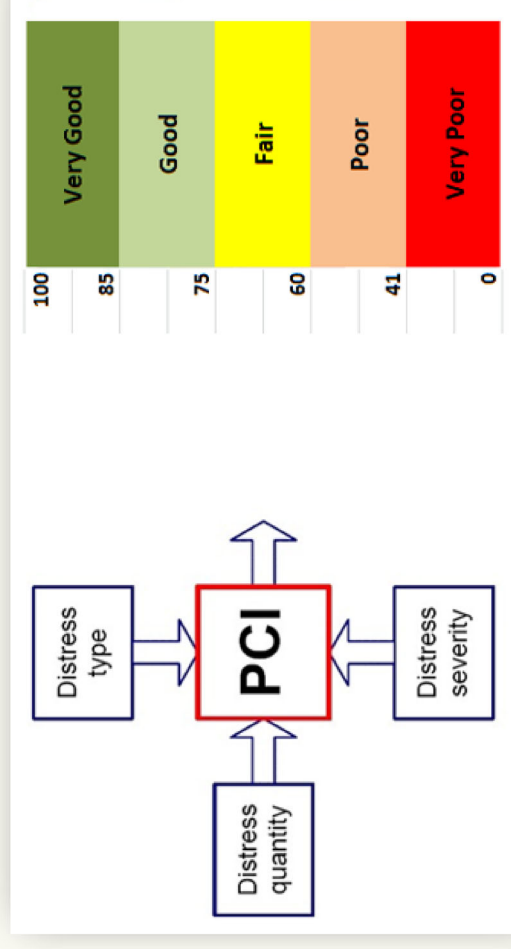


## HOW THE PAVEMENT MANAGEMENT PROGRAM IS UPDATED

- **Assessment of Existing Pavement Network (segmentation review)**
- **Update of Work Histories**
- **Condition Survey (USACE walking field inspections)**
- **Develop Rehabilitation Strategies / Life-Cycle Analysis**
- **Forecast Future Pavement Preventive / Rehabilitation Costs**
- **Develop 5 & 7 -Year Plans based on Available Budget and Desired Goals**

## GENERATING PAVEMENT CONDITION INDEX (PCI)

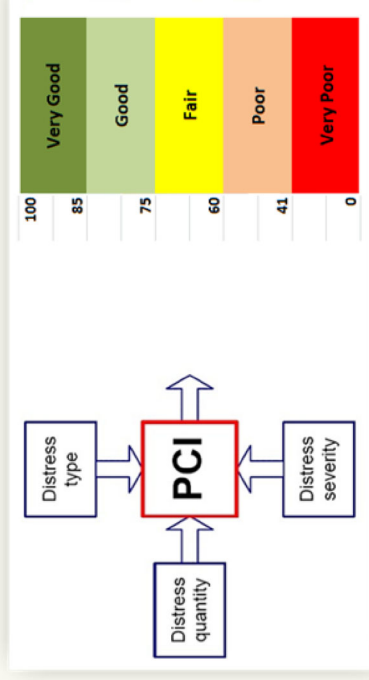
- **Walking Condition Surveys completed June 2025**
- **True Area Calculations were performed to improve PCI accuracy and financial estimates**
- **Quality Control: Re-survey 10% of segments**
- **Results generated a PCI for each road segment**
- **PCI is a condition rating from 0 to 100**



# PAVEMENT CONDITION INDEX (PCI) RESULTS

## Glendora PCI

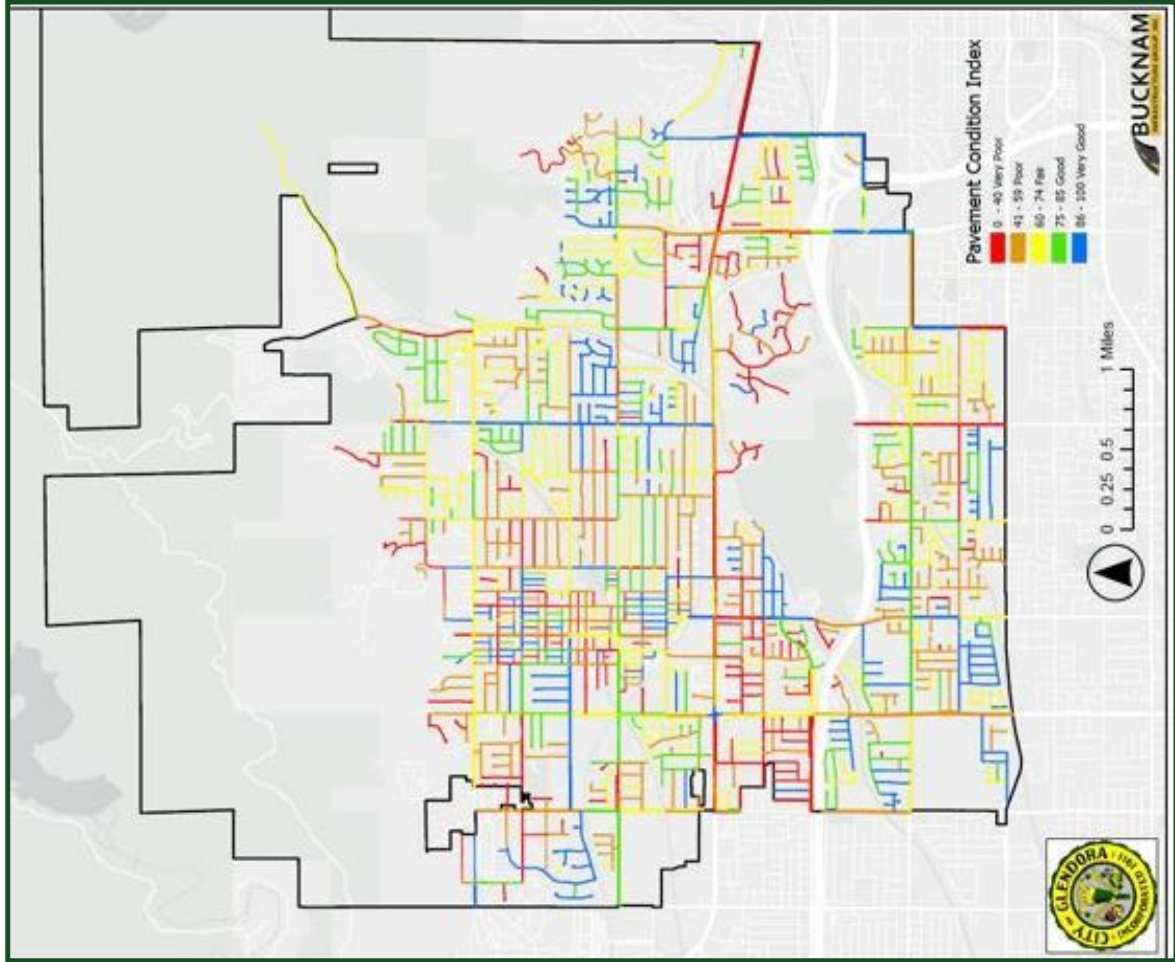
- 2025: 63.2
- 2020: 65.4
- 2018: 59.0
- 2015: 62.5



Sustained PCI condition level of “Fair”

## Condition Distribution by Section Mileage for all Streets.

Condition	PCI Range	Art / Coll	% of Art / Coll	Local	% of Locals	Alleys	% of Alleys	Total	% of Network
Very Good	86-100	6.7	15%	17.6	16%	2.3	37%	26.6	16%
Good	75-85	6.9	15%	18.4	16%	0.6	10%	25.9	16%
Fair	60-74	11.5	26%	24.7	22%	0.4	6%	36.6	22%
Poor	41-59	11.8	26%	34.1	30%	0.9	15%	46.8	29%
Very Poor	0-40	8.1	18%	17.4	16%	2.0	32%	27.5	17%
		45.0		112.2		6.2		163.4	



# PCI COMPARISON

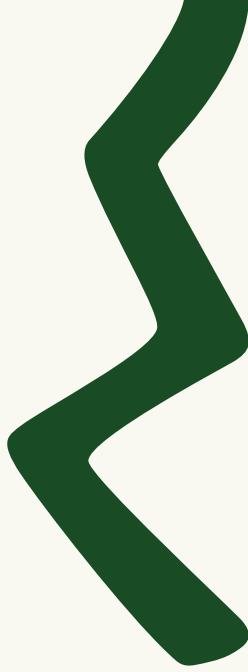
## Glendora compared to other LA County cities:

Neighboring City PCI's (Citywide 2024 & 2025 PMP's)			
Alhambra	76.0	Duarte	77.3
South Gate	55.5	Monterey Park	65.7
Lynwood	71.6	Covina	73.4
South Pasadena	63.8	Commerce	65.5
El Segundo	80.3	Compton	58.0
Sierra Madre	76.5	Gardena	81.2

Weighted LA County PCI: 66

CA State-wide PCI: 65

**2025 PCI of 63.2 places Glendora in the "Fair" range, similar to Statewide and Countywide averages.**



# MAINTENANCE & REHABILITATION STRATEGIES

## Global Maintenance

- General Repairs
  - Crack Sealing, Patching:
- Slurry Seals:
- Cape Seals:

## PCI Range

20 – 95  
65 – 85  
40 – 65

## Major Rehabilitation

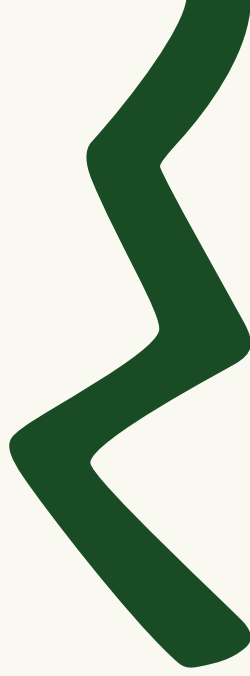
- Overlays:
- AC Reconstruction:

## PCI Range

20 to 65  
0 to 20

## **Minimum Level of Service = 65**

This value will trigger decisions between preventative maintenance vs. rehabilitation



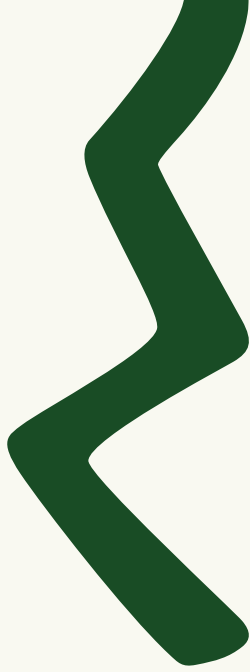
# FUNDING SCENARIOS EVALUATED

## Forecasted Pavement Condition based on four (4) Funding Scenarios:

Program	PCI Target	# of Years	Average Annual Investment	Total Program Investment
Maintain 63 PCI	63	5	\$5.58 M	\$27,916,500
Reach PCI 65	65	5	\$6.48 M	\$32,406,800
Reach PCI 67	67	5	\$8.38M	\$41,902,200
Reach PCI 70	70	7	\$8.42M	\$58,946,700

## Funding scenarios took into consideration the following:

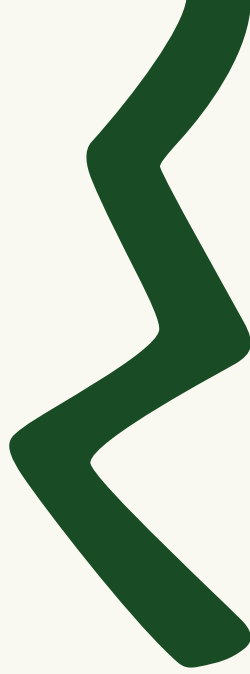
- Long Term Goals
- Alternative Pavement Applications
- Maintaining “preventative maintenance” condition
- Deferred maintenance/backlog balance



## FUNDING SCENARIO: REACH PCI 67

Plan Year	PCI	Slurry Seal	Overlay	Funds (\$)	Deferred Maint.
2025 PMP	63.2				\$46,785,500
2025 - 2026	65.0	\$353,900	\$8,077,900	\$8,431,800	\$45,981,800
2026 - 2027	66.0	\$1,498,000	\$6,712,800	\$8,210,800	\$37,202,800
2027 - 2028	67.0	\$1,250,400	\$7,123,400	\$8,373,800	\$41,467,500
2028 - 2029	67.0	\$797,500	\$7,602,800	\$8,400,300	\$51,084,600
2029 - 2030	67.0	\$522,100	\$7,963,400	\$8,485,500	\$71,951,900
		\$4,421,900	\$37,480,300	<b>\$41,902,200</b>	

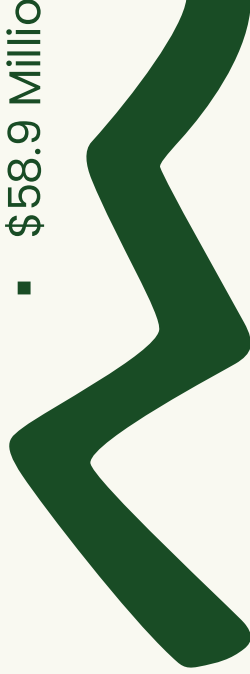
- Five Year (5) Program: FY 2026 – FY 2030
- \$8.38 Million Annually
- \$41.9 Million Five Year Total



## FUNDING SCENARIO: REACH PCI 70

Plan Year	PCI	Slurry Seal	Overlay	Funds (\$)	Deferred Maint.
2025 PMP	63.2				\$46,785,500
2025 - 2026	65.0	\$353,900	\$8,077,900	\$8,431,800	\$45,981,800
2026 - 2027	66.0	\$1,498,000	\$6,712,800	\$8,210,800	\$37,202,800
2027 - 2028	67.0	\$1,250,400	\$7,123,400	\$8,373,800	\$41,467,500
2028 - 2029	68.0	\$951,400	\$7,981,800	\$8,933,200	\$47,761,500
2029 - 2030	70.0	\$859,700	\$8,451,100	\$9,310,800	\$56,642,600
2030 - 2031	70.0	\$678,600	\$7,134,500	\$7,813,100	\$61,353,900
2031 - 2032	70.0	\$651,000	\$7,222,200	\$7,873,200	\$62,987,600
		\$6,243,000	\$52,703,700	<b>\$58,946,700</b>	

- Seven (7) Year Program: FY 2026 – FY 2032
- \$8.42 Million Annually
- \$58.9 Million Seven Year Total



# FUNDING GAP

Funding Gap Summary "Reach 67"			
Fiscal Year	5 Year CIP & Staff Budgeted Amount*	Funds Needed to Reach PCI 67	Additional Amount Required to Reach PCI 67
<b>FY 2026**</b>	\$9,446,814	\$8,431,800	(\$1,015,014)
<b>FY 2027</b>	\$5,554,872	\$8,210,800	\$2,655,928
<b>FY 2028</b>	\$7,967,482	\$8,373,800	(\$406,318)
<b>FY 2029</b>	\$1,944,755	\$8,400,300	\$6,455,545
<b>FY 2030</b>	\$2,049,000	\$8,485,500	\$6,436,500
<b>5 Year Total:</b>	\$26,962,923	\$41,902,200	\$14,939,277
<b>Avg/Year:</b>	\$5,392,585	\$8,380,440	+\$2,987,855 / year

\* Includes \$500,000 in Reactive Repairs/Maintenance  
 \*\* Includes Carry Forward

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<b>FY 2026**</b>	\$9,446,814	\$8,431,800	(\$1,015,014)
<b>FY 2027</b>	\$5,554,872	\$8,210,800	\$2,655,928
<b>FY 2028</b>	\$7,967,482	\$8,373,800	(\$406,318)
<b>FY 2029</b>	\$1,944,755	\$8,933,200	\$6,988,445
<b>FY 2030</b>	\$2,049,000	\$9,310,800	\$7,261,800
<b>FY 2031</b>		\$7,813,100	\$7,813,100
<b>FY 2032</b>		\$7,873,200	\$7,873,200
<b>7 Year Total:</b>	\$26,962,923	\$58,946,700	\$31,983,777
<b>Avg/Year:</b>	\$5,392,585	\$8,420,957	+ \$3,028,343 / year

\* Includes \$500,000 in Reactive Repairs/Maintenance  
 \*\* Includes Carry Forward

An additional \$3 Million is required to "Reach 67 PCI."



# FUNDING SOURCES

Revenues Available by Fund				
Fund	FY 2024*	FY 2025**	FY 2026**	FY 2027**
Gas Tax	\$2,079,738	\$2,311,500	\$2,038,400	\$2,133,400
Proposition C	\$1,175,566	\$1,230,100	\$1,136,760	\$1,136,760
RMRA	\$1,404,619	\$1,377,800	\$1,411,540	\$1,411,540
Measure M	\$997,250	\$1,045,100	\$949,948	\$949,948
Measure R	\$884,576	\$908,800	\$833,370	\$833,370
CDBG	\$61,061	\$461,509	\$444,625	\$275,000***
STP-L	\$4,021	\$3,200	\$3,000	\$3,000
<b>Total:</b>	<b>\$6,606,831</b>	<b>\$7,338,009</b>	<b>\$6,817,643</b>	<b>\$6,743,018</b>

\*\*Projected Annual Revenues per the 2025 – 2027 Adopted Two Year Budget  
 \*Actual Revenue  
 \*\*\* Estimated

Voter-approved Measures E(FY2019) and Z(FY2025) have strengthened overall City finances, contributing to essential City services, capital improvements, and staffing. Measures E & Z may be considered as potential supplemental sources for future street improvements.



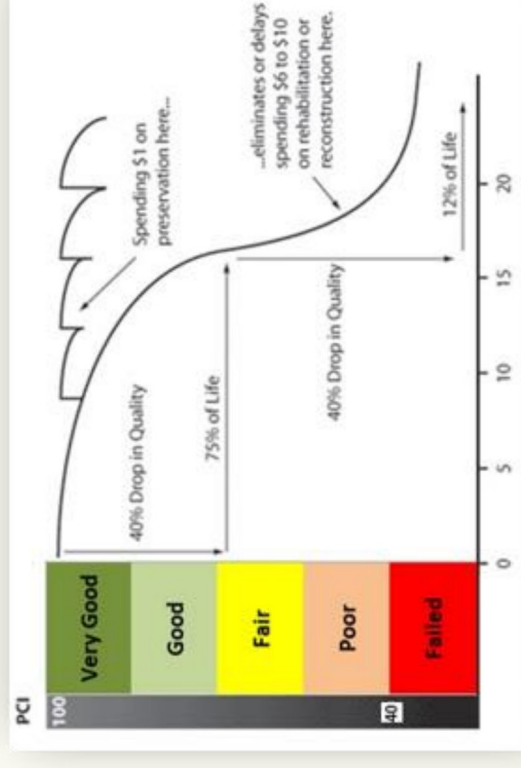
## FUNDING GAP

- Maintaining an annual investment near \$8.4M allows the City to improve PCI, slow deterioration, and reduce long-term costs.
- Current pavement funding averages \$5.4M/year, creating an annual \$3M shortfall compared to what is needed to reach PCI 67–70.
- Investing at the higher level now is fiscally prudent—early maintenance avoids the exponentially higher costs of deferred repairs.
- Planning for consistent \$8.4M/year through FY 2031–32 positions the City to reach target PCI goals and preserve its \$439M pavement network

## WHY TIMELY MAINTENANCE MATTERS

- Pavement naturally deteriorates over time - maintenance timing is critical.
- Preventative treatments applied early dramatically extend pavement life.
- Streets in “good” condition are often treated first because they provide the best return on investment.
- Waiting until pavement becomes “poor” or “failed” requires expensive reconstruction.
- Every \$1 spent on early preservation avoids \$6 - \$10 in costly rehabilitation later.
- Strategic, timely maintenance helps preserve the overall condition of the City’s network.

*“Good streets are cheapest to preserve and provide the highest ROI.”*



## KEY FINDINGS

- **Current weighted PCI: 63.2, maintained an overall “Fair” network**
- **32% of streets are in “Good” to “Very Good” condition**
  - Long-term goal: increase “Good to Very Good” condition to 50% of the system
- **53% of the network (86.6 miles) now requires overlay or reconstruction**
- **Network stability is largely due to recent arterial investments**
- **Majority of local streets show increasing need for rehabilitation as they age**

## RECOMMENDATIONS

- **Establish a Citywide Goal of Achieving PCI 67 within the next 5 years**
- **Recommend Average Annual PMP Funding of \$8.4M**
- **Use Balanced Approach to Repairing Roads in the Lower / Mid PCI tiers**
- **Reassess PMP budget projections every two years**
- **Perform Citywide pavement inspections every three years**
- **Evaluate alternative pavement applications to extend pavement life and maximize dollars.**
- **Continue leveraging water and street project coordination to maximize dollars.**

## SUMMARY

- **City's pavement network maintains a "Fair" PCI**
  - 2025 PCI of 63.2 a slight change from 65.4 in 2020
- **Long-term trend shows improvement:**
  - PCI was 62.5 (2015) and 59 (2018), before recent investments.
- **Earlier assessments (2015 & 2018) did not include alleys.**
  - Inclusion of alleys would have slightly lowered those PCI scores
- **Maintaining stability requires significant ongoing investment;**
  - Without recent work, PCI would have declined into "Poor" range
- **Upcoming Water and Street Improvement Projects (Colorado, Cypress, Glendora, Lone Hill) will address nearly 1,000,000 sq. ft. of pavement, Significantly Strengthening Future PCI.**

## NEXT STEPS

- **Continued investment at \$8.4M annually is required to move from “Fair” to “Good”**
- **Updated PMP provides a data-driven framework for prioritizing maintenance and rehabilitation**
- **Next Steps:**
  - Align PMP recommendations with the Water Master Plan
  - Coordinate both plans with the City's 5-Year CIP
  - Identify timing opportunities where water + street projects can be combined for greater efficiency
  - Return to Council with funding recommendations consistent with the target PCI goals

**RECOMMENDATION:**

**THAT THE CITY COUNCIL:**

1. Receive and file the 2025 Pavement Management Program Update prepared by Bucknam Infrastructure Group, Inc.

